



## Late Model 2025 RULES AND

### REGULATIONS

The interpretation and application of all rules contained herein, as well as, all amendments, supplements and revisions that may be implemented shall be at the sole discretion of The Anderson Speedway Committee and will be final and binding.

The Rules and Regulations set forth herein are designed to provide drivers and crew members with a safe, clean and competitive racing atmosphere. Every attempt will be made to control the costs of racing within reason. It is your responsibility to read and understand the rules and abide by them. Assuring that your equipment, your car and your personal safety habits are up to standard.

All Rules and Regulations apply to each and every driver, mechanic and/or pit personnel. Every driver, mechanic and/or pit personnel are expected to read and understand rules set forth. If you have any questions, please use the website for all your questions. All inquiries will be answered within a reasonable amount of time.

These rules are subject to change to ensure fairness in competition. Any car that does not conform to all Rules may be allowed to compete with a weight penalty levied by AMS Tech Official. This will be done at the discretion of AMS Officials.



### ON TRACK INCIDENT PROCEDURE

This procedure has been posted as a separate rule/procedure for the safety of all the Driver's and Crew Members. Please be sure to read, understand and abide by the following rule/procedure. Failure to abide by said rule could result in disciplinary action at the discretion of the Race/Competition Director.

During any race event, if a race car is involved in an on-track incident and/or is stopped on or near the racing surface and is unable to continue to make forward progress, the Driver should take the following steps. ONLY under extenuating emergency conditions with the race car (i.e. fire or smoke in the cockpit) should a Driver ever exit the race car. It is mandatory that you stay in the race car until otherwise directed by Safety Personnel or until the race car and Driver has been taken to their own pit area.

If uninjured, shut off electrical power and lower the window net.

DO NOT loosen, disconnect or remove any Driver personal safety equipment until directed by Safety Personnel or Track Official.

After being directed to exit the race car, the Drivers should proceed to either the EMT or as otherwise directed by Safety Personnel or Track Official.

Mandatory, at NO time should a Driver or Crew Member approach any portion of the racing surface

or apron.

At NO time should a Driver or Crew Member approach another moving vehicle.

All vehicles not involved in the incident or that are able to proceed with forward progress should proceed at a cautious speed. Use extreme care as they approach the on-track incident scene and follow any/all directions given by Safety Personnel or Track Officials. Cars in line behind the Safety car should be single file and should NOT weave or otherwise stray from the line in the vicinity of the incident.

We will qualify for position in all events.



#### **GENERAL RULES**

1 The driver will be the sole spokesperson and responsible for their car and team members in any and all matters pertaining to the series. At all events the driver assumes responsibility for the actions/conduct of all team members, whether they are in the pits or in the grandstand area spotting. Verbal abuse, cursing or other threatening actions will result in the ejection of the violator and possible disqualification of the car associated with the violator. Physical abuse will result in the ejection and possible arrest of the violator. The driver is responsible for any unpaid fines for crew members. Any participant that has outstanding fines will not be allowed to attend any AMS event until fines are paid.

3 It is mandatory that all drivers/spotters attend the drivers meetings. Failure to attend will result in starting in the rear of the field. There will be a roll call.

4 Notice must be given to the Race Director and Series Administrator of any driver changes prior to the start of the event. No driver changes may be allowed once the event has been started. Failure to report any driver changes will result in disqualification and no points or monies will be awarded for that event.

5 Notice must be given to the Competition Director and Series Administrator of any motor combination changes prior to the start of the event.

Failure to report any motor combination changes could result in possible loss of contingency awards, points and/or monies to be awarded for that event.

6 When a driver refuses to participate because of a dispute with the rules, length, type or style of the race, the driver will be subject to suspension for a period determined by the Race Director.

7 Anyone injured during the course of the event must notify the Series Administrator. The injured must provide all required information prior to leaving the premises on that date in order to be eligible for any insurance benefits.

8 The possession, consumption or distribution of ANY alcoholic beverage or controlled substance at any time while in the pit area is strictly prohibited. Any drivers or crew members found using or possessing alcohol or controlled substances will be ejected from the event and suspended for the remainder of the season.

9 No weapons, concealed or otherwise, are allowed at any time. The display of any type of weapon or threat of bodily harm will result in permanent suspension for the entire race team and will result in the arrest of all parties involved.

10 All drivers must provide a W-9 form and a Driver Profile to the Series Administrator. Both must be on file before qualifying in order to receive pay for that event.

Prize money for support classes not collected on race night may be picked up at the next scheduled race event. We will not be mailing any money.

11 Race/Competition Director Reserves The right to enforce, update, revise or cancel any of the rules stated herein for the betterment of the Series.

12 AMS demands courteous conduct from all participants at all times. The Series will not tolerate profanity in front of race fans, Officials or

Management. The Series will not tolerate profane signs on your car and/or clothes. Professional behavior and representation by all teams is required by tracks hosting our events. The Series will also not tolerate slander to the Series or any of its Officials at any time. This will also apply to Social Media or otherwise. Slander will cost you either points and/or monies or could subject you to fines and/suspension. The severity of the situation will dictate disciplinary action and this will be decided upon by the Series Board.

- 13 We will have contingency sponsors this season. In order to qualify for **ANY** products or money, you will be required to post **ALL** decals on the car at each race event. Decal packages will be an all-on-one sheet and must be posted right behind the front tires on the fender/door. It is your responsibility to keep **ALL** these decal packages as a whole complete set and on your car to be eligible for **ANY** of the contingency awards. If you need replacement decals, please see Diane at the Series trailer.
- 14 The Series Require each race car to display the make of vehicle by way of decals on the front nose piece. Whatever make of vehicle you claim on your paperwork must match the decal package on your nose piece. Should you elect to change the make of vehicle, you must notify the Series Administrator of your change. Failure to report said change, could result in loss of contingency awards, points and/or monies.
- 15 Any questions that you may have must come through one of the following avenues. Either the AMS Facebook Messenger or look on the webpage for the email address of the person you need. All inquiries will be answered within a reasonable amount of time.
- 16 No one under 12 years of age will be eligible to compete in the Limited division.
- 17 If you are 12-18 years of age, you must have appropriate parental consent forms signed by both parents/guardians and notarized. See Series Administrator for forms. Minors must have qualified references to be eligible to participate. You may also be asked by the Race Director to perform a certification test. This would be at the discretion of the Race Director. The Driver could be subject to pay a certification fee of \$100.00.
- 18 The above said rules have been set by the AMS Committee.  
Each race facility will also have rules in place. Please make yourself and your team aware of any and all rules set by each race facility we visit. These would be rules pertaining to scheduling and what will be tolerated at their facility. Basic Conduct Rules and Procedures will apply to us as we visit each track. We are their guests and we all need to be aware of our conduct and manners. Let's make a great impression at each event so that we are welcome to come back.
- 19 Notice must be given to the Competition Director of any changes made to a car that would result in a weight difference based upon original information turned in on Driver Info form. Changes pertaining to engine type, overbore of the engine and wheelbase. Notice must be given to the Series Administrator upon race registration. Failure to report changes will result in disqualification and no points or monies will be awarded for that event.
- 20 All Drivers are required to fill out necessary paperwork before being eligible to participate in any Limited division race event. This would include Driver Info, W-9, Driver and Crew Chief License, Rookie qualification, Parental Consent and Race Entry/Transponder Rental forms. All fees must be paid in full before being eligible to participate in any Anderson Motor Speedways event.

The above said rules have been set by the AMS Committee. All rules are subject to change by the Committee at any time, but drivers will be notified of any changes.



## SAFETY RULES

- 1 Antifreeze - Any car caught running antifreeze will be fined \$100.00

**2 Seats and Seat Belts** - Each truck must be fitted with a SFI approved seat and safety release harness(***SFI approved Belts ONLY - no more than 5 yrs. old-label must be visible***) . Minimum 2" shoulder harness width and 3" lap belt width. Harness must come from behind the driver and all belts must be securely fastened to the roll cage. No homemade seats are permitted. All seats and harnesses must pass the Race/Competition Director's inspection.

**3 Helmets**- Each driver is required to wear a full-face helmet any time the race truck has entered onto the racing surface. ***It is mandatory that all helmets conform to Snell SA 2010 Standards for Protective Headgear as they apply to competitive automotive sports and have a sticker visible for inspection. NO Motorcycle helmets!!! The Anderson Motor Speedways Series recommends that Drivers use the Head and Neck Restraint device.***

**4 Driving Suits** - Full coverage (covering the body from neck to ankle) driving suits are mandatory. 1 or 2 piece fire retardant suits of Nomex or equivalent materials are mandatory. Double layer or more is highly recommended. ***Fire suits must be labeled as "Fire Retardant"***. Fire retardant gloves, socks, shoes and undergarments are strongly recommended. A full fire suit and helmet must be worn at any and all times the truck is on the racing surface. No Exceptions!

**5 Fire Extinguishers** - Each car must be equipped with a fully charged fire extinguisher that has been approved by the Race/Competition Director. NO Exceptions! It must be mounted within easy reach of the driver. Extinguishers must be properly installed with appropriate metal brackets and screws. The extinguisher must be in proper working condition and have a functioning readable gauge. On board fire systems are highly recommended.

**6 Window and Nets**- All cars must have front and rear window shields. All cars must have a safety net covering the drivers door window area and the net must be secured in place at all times the car is on the racing surface. Window net must have an SFI tag. No exceptions! NO fish type nets are allowed.

**7 Roll Bars** - Anderson Motor Speedways Series approved full roll cage is required. ***It is required that roll bars within the Driver's reach be padded with material (SFI-45.1) designed for roll bar use.*** Pipe insulation will not be allowed. All roll cages must pass safety inspection done by the Race/Competition Director prior to being allowed to compete in an event.

**8 Fuel Cells** - A fuel cell is mandatory in all cars. The fuel cell must include a puncture resistant bladder, internal form filler and be installed in a minimum 18 gauge (.047 inch) steel container. No additional or separate fuel container will be allowed. No electric fuel pumps are allowed. Fuel cells will be approved at the discretion of the Competition Director.

**9 Battery** - Battery may not be installed within the driver's compartment. Battery must be securely bolted down .

**10** No person is allowed to ride on the outside of any race car at any time.

**11 Car Inspection** - All cars are subject to a safety inspection at any time prior to taking part in any event. If the Race/Competition Director deems a car has not met all safety guidelines, that car will not be allowed to compete until all infractions have been corrected. It's The responsibility of the driver, car owner and crew members, to have the car free of mechanical defects, in a safe racing condition and properly inspected prior to an event.

The above said rules have been set by the Anderson Motor Speedway Series Committee. All rules are subject to change by the Committee at any time, but drivers will be notified of any changes.



## PIT RULES

- 1 Any fighting or reckless driving in the pit area or on the race track will subject the offender to suspension. The seriousness of the incident will be determined by the Race Director. Any other offenses from the said individual will result in fines, suspension or both from AMS.
- 2 Any person entering into a pit area other than their own where a disturbance of any kind occurs will be considered to be at fault. Stay on your own with your own!
- 3 Fighting will not be tolerated anywhere on the speedway grounds or access roads, no matter what the provocation. Rough driving, swearing, obscene language and/or gestures will not be tolerated and will be regarded as unsportsmanlike conduct and subject to disciplinary action. If fighting should occur, both parties and/or crew will be subject to immediate ejection.
- 4 Pit Road Speed will be monitored. During practice, the 1st offense will be a Verbal Warning. The 2nd offense will end your practice for the day. For the race event, 1st offense will be Verbal Warning and 2nd offense will result in losing a lap. Your 3rd offense will end your race for the night.
- 5 When in the pit area, maintain a constant watch for your own protection. Drivers and/or crew members operating race cars in the pit area shall do so maintaining a safe speed. You will follow the traffic pattern set forth by the Race Director. Failure to comply will result in disciplinary action set forth by the Race Director.
- 6 All drivers must line up their cars in the staging area designated by the Race Director immediately when asked. Anyone who fails to comply in a timely manner is subject to starting in the rear of the field.
- 7 No weapons, concealed or otherwise, are allowed at any time. The display of any type of weapon or threat of bodily harm will result in permanent suspension for the entire race team and will result in the arrest of all parties involved.

The above said rules have been set by the AMS Committee. All rules are subject to change by the Committee at any time, but drivers will be notified of any changes.



## FLAG RULES

- 1 Any driver who does not obey the flag rules will be subject to lap penalties, time penalties, disqualification and/or fine.
- 2 **Green Flag:**  
Displayed to start the race. CARS MUST RETAIN POSITION UNTIL THE GREEN FLAG IS DISPLAYED.  
On restarts only, the green flag will be displayed simultaneously as the yellow light goes out and the race will resume.  
For all starts and restarts, passing to the left before the start/finish line is prohibited.

- 3 **Yellow Flag:**

Signifies caution and this flag will be given by the Flagman. Cars must slow down to a CAUTIOUS PACE and HOLD POSITION until the green flag is once again displayed.

#### **4 Red Flag:**

Danger. Stop as quickly as possible, regardless of position of cars on track or until otherwise instructed by a Race Official.

#### **5 Black Flag:**

Pull off track immediately into the pit area for a consultation with a track official. Failure to obey black flag will result in disqualification of the car, suspension and/or fine.

#### **6 Blue/Yellow Flag:**

Move over the flag. Signifies that faster traffic is overtaking cars being given the flag. Cars given this flag should be prepared to yield to overtaking traffic and move to the inside of the race track.

#### **7 White Flag:**

One lap to go. When this flag is displayed, it means that the leader has started the last lap.

#### **8 Checkered Flag:**

End of race. When the leader is given the checkered flag, the balance of the field receives the checkered flag in the same lap.



### **RACE PROCEDURES**

1 Check your Schedule of Events. Be aware of where you need to be and at what time. All Schedules are subject to change depending on the Management of the track visited. Drivers will be notified of any changes that are made.

2 It is mandatory that all drivers/crew chiefs/spotters attend the drivers meetings. Failure to attend may result in starting from the rear of the field. There will be a roll call.

3 According to the posted race day schedule, if you are late for any required event pertaining to that schedule, penalties may be applied.

4 All cars participating in a Limited event must have a spotter. Allspotters will be required to wear the number of the car they are spotting, easily recognizable from behind. All cars/spotters must have working 2-way radios.

5 All spotters will be assembled in a designated area in the grandstands for the race. It is the spotters responsibility to be in the designated area prior to race start. Any car that does not have a spotter in the designated area will not be allowed to race. It is the drivers responsibility to designate a spotter for their car. Roll will be taken.

7 The pole position car will bring the field from the designated staging area to the front stretch where driver introductions will be made.

8 Each event will begin with five warm up laps. The flagman will always start the race. 9 Leader must complete one green flag lap before it is considered to be an official start.

If the yellow or red flag comes out before the first official lap is completed, cars will return to their

original starting position. The only exception would be cars/involved in bringing out the yellow. Those cars will restart from the rear of the field.

10 For all starts and restarts, passing to the left before the start/finish line is prohibited. The only exception will be if the car in front of you fails to accelerate due to a mechanical issue. At that point, you will be allowed to pass in order to avoid an accident. All starts will be determined as a good start by the Race Director. Should it not be deemed a good start, you will be notified by your spotter.

11 On all starts and restarts, jumping the flag is prohibited. One warning will be issued, a second offense will result in being placed in the rear of the field.

12 All restarts will be double file. The leader will set a reasonable pace for all restarts. There will be NO brake checking throughout the field. Driver/s will be given only one warning and on next occurrence Driver/s will be black flagged and will restart from the rear of the field. The Leader will have the option of choosing inside or outside lanes for restarts.

13 Drivers WILL NOT race back to the flag when a yellow condition is displayed. Slow your pace as soon as your spotter and/or you see the yellow has been displayed.

14 Under yellow conditions, cars will restart in order of the last green flag lap completed according to official scoring. Cars involved with bringing out the yellow condition will restart from the rear of the field. If drivers should dispute the starting position given, the Driver must pull to the side of the car where they feel they should be restarting. Remain there until a decision is made by the Race Director. It is the Driver's Responsibility to make Officials aware that the starting lineup is in question. Make sure you do this before the one to go signal has been displayed. Once displayed, your opportunity for change is not valid.

15 All cars that make any contact, as a result of an accident, will restart from the rear of the field. Any car/involved in bringing out the yellow or red condition will restart from the rear of the field. Any car/s that spin out or stop without making contact, in an attempt to avoid an accident that has already brought out the yellow, will restart in the position they were running in on the last lap completed. This will be determined by official scoring and the Race Director. All car/s restarting from the rear because of a yellow condition will be lined up in the order they drive away from the accident.

16 When the leader takes white flag, the race is officially over. Anyone involved in the caution, will be scored at the tail end of the last lap completed. If you turn the leader on the last lap, you will be scored at the tail end of the last lap completed.

17 Any car stopping on the track, in an attempt to bring out a yellow condition, will restart from the rear of the field. Second occurrence, will result in the scoring card being pulled and no more laps will be counted or credited.

18 Any car spinning out or bringing out the yellow or red condition three times during an event will be parked for the remainder of that event.

19 Any car/stat spin out will restart from the rear of the field.

20 Caution laps do not count. Any car that goes into the pit area under yellow conditions, will restart at the tail of lead laps cars without losing a lap, provided the race has not been restarted.

21 All races with 10 or more cars will be a 75 lap race unless a special event

22 Slower cars will be given the move over flag. Cars that refuse to acknowledge the move over flag will be black flagged and given a stop and go penalty.

23 Anytime a lapped car makes contact with the lead car and causes the lead car to wreck, the lead car and only the lead car will retain his position. Should the lead car need to pit, he forfeit that position to the second place car.

- 24 No deliberate beating, banging or rough driving will be tolerated. First offense will result in being moved to the rear of the field. Second offense will result in being black flagged to the pit area and held for one lap. Third offense will result in being parked for the remainder of the event. Incidents under caution conditions, will result in being parked for the remainder of the event and possible ejection from the premises.
- 25 Underrated flag conditions, no one other than drivers, track Officials, EMT workers or AMS Officials are allowed on the track or past the pit wall. Drivers should stay in their cars unless directed otherwise. Cars caught on pit road under a red flag condition will not be penalized for working on their cars.
- 26 Under red conditions, cars on the track will remain on the track until directed by an Official that it is safe to move on to the pit area.
- 27 Drivers must remain with their cars anytime it is on the track and/or should require assistance from the wrecker service. Drivers must assist wrecker service as to where to hook your car and where your pit is located. Be courteous.
- 28 The Limited division will make three (3) attempts to finish under green. After the third attempt, the race will end and be scored based on the last official lap completed.
- 29 At the completion of each event, the race winner will report to the front stretch for interviews and photos. The second, third and fourth place car should report to the tech area along with the random pick of the night.
- 30 Once the winner is parked in victory lane, no one is to cross the wall before AMS Official is present and releases you to proceed.
- 31 As you continue on to either your pit area or the tech area, be aware of your speed and surroundings. DO NOT speed or spin out on pit road. There will be a lot of activity and people on pit road. There will only be one warning for this offense for the year and on the second occurrence you will be fined.
- 32 Only the top four and the random pick will be teched each event unless otherwise directed by the Race/Competition Director.
- 33 No drinks, tools or anything that was not already in the race vehicle during the race event will be allowed in the race vehicle as you go through post tech. If caught with anything inside a race vehicle that wasn't raced, you will be DQ'd.
- 34 Only the driver and 2 crew members will be allowed in the tech area at one time. The driver may substitute another crew member should he/she desire to leave tech momentarily.



## **PROTEST PROCEDURES**

1 The AMS Speedway reserves the right to inspect any car at any time. It is the drivers responsibility to have the car available for inspection by the Anderson Motor Speedways Competition Director or any other appointed AMS Official.

2 Protests must be made in writing within five minutes of race completion and turned in to the Competition Director ONLY. All protests are CASH ONLY. Protests must be filed by either

the car owner or the driver ONLY.

3 NON-ACCEPTANCE OF A PROTEST by SEST: The protest may, at the discretion of the Competition Director, be declined if deemed to be malicious or spiteful. AMS Competition Director may also penalize the author of a protest deemed to be malicious, spiteful, or who otherwise acts in bad faith.

4 Protest fees must be turned into the Competition Director at the time the protest is filed. Protest fees are as follows:

5 Any parts and/or areas being protested or claimed will be overseen by AMS Official. Series will allow the protester and one other person of their choice for inspection. No parts should be removed without AMS Official present and authorized. There will be no intentional destruction of any claimed and/or protested parts/items during the removal process. If deemed intentional by AMS Official, result will be disqualification and no points or monies will be awarded. Parts will be confiscated.

Cylinder Head and Intake removal: \$500.00

Bottom end of engines will be an additional \$1500.00

6 Only the top five finishers will be eligible to participate in the protest procedure. The car being protested must finish in front of the protester. Protesters may only protest up to two positions in front of their own.

No more than two (2) items can be protested at one time.

Also, you may not protest more than two (2) consecutive weeks.

If the car is found to have an illegal part, it must be shown that the part has been removed before the start of the next race event.

AMS Officials reserve the right to refuse any protest and at any time.

7 While taking protested parts off, if the AMS Official finds any other illegal parts while getting to the protested part, this too will lead to disqualification. The protest will still continue to the protested part/s.

8 Only the protested driver and two assistants are allowed in the tear down area to assist with the inspection process.

9 Any car found to be illegal will forfeit all prize money and all points earned for that event. Any car that fails to tear down for inspection, either under regular post race tech or protest conditions, will forfeit all prize money and all points earned for that event.

10 The AMS Speedway reserves the right, in all events, to tear down the next car in position until a legal car is found. This will be at the discretion of the Competition Director.

11 All CLAIM RULES have the same requirements as Protest procedures. CASH ONLY at time of filing written protest and turned in to the Director of Competition within twenty minutes of race completion.

The above said rules have been set by the AMS Committee. All rules are subject to change by the Committee at any time, but drivers will be notified of any changes.



## LATE MODEL RULES

**All rules and regulations stated herein are subject to the interpretation of the AMS Tour officials. The AMS reserves the right to add, delete or modify any rule(s). The AMS Director and/or Tech Director shall be empowered to permit minor deviations from any of the specifications or impose any further restrictions that in his/her opinion does not alter the minimum acceptable requirements.**

### 2025 LATE MODEL STOCK CAR TECHNICAL DATA

#### Safety

1. Radio communication to the drivers is mandatory, with a minimum of one (1) spotter for each team. **Spotter must have the capability to monitor series race control with a scanner.**
2. SFI rated seat belts and double shoulder harness will be required. SFI or FIA rating and expiration date label must be visible for inspection. Seat belts and harnesses must NOT be used after the date on the SFI/FIA label. A crotch strap will be required.
3. Driver seats and headrests/head surround assembly are recommended to have an SFI approved rating of 39.1. Seats must be mounted to the frame and roll cage of the vehicle.
4. It is highly recommended that all seats are equipped with padded leg extensions on both the right and left sides. These extensions should be securely mounted to the seat and the vehicle structure. These extensions, as well as any seat padding, seat inserts and seat upholstery should be constructed of flame-retardant material.
5. A left side intrusion plate for the driver's door will be mandatory. Must be 12 inches (12") high post-to-post, 1/16" minimum thickness steel or and must be fastened with a minimum of six (6) half-inch bolts or securely welded to series' approval. The interior area of the vehicle must be completely enclosed. Any device or duct work that permits air to pass from one area of the interior to another, or to the outside of a vehicle will not be permitted.
6. The Driver's window must be equipped with a safety net with a quick release latch. String window nets will not be permitted. The minimum net size must be 22" wide and 16" high. When latched, the window net must fit and pull tight.
7. Resilient padding designed for roll bar use must be installed on any roll cage member which can be reached by any extremity of the driver while driver is normally seated with restraints fastened. The steering wheel must be padded.
8. All competing teams MUST possess a minimum of 10 lb. aluminum working fire extinguisher while in attendance in pits, and this item must be presented at inspection. Car number must be painted on the fire extinguisher.
9. A working Fire Suppression system or driver accessible fire extinguisher is required. All cars must have a fuel quick disconnect or SRI Performance vacuum style fuel shut off placed at the point where the fuel exits the cell. No manual shutoffs

10. All windshields (front, side, and rear) of the car must be made of Lexan and be clear in appearance all the way around the car and be flush with the mounting bed. NO tinted Lexan permitted.

11. The use of any part that is totally or partially made of tungsten is NOT permitted.

## **Eligible Cars and Body Guidelines**

### **2007-2019 AR BODIES/FIVE STAR BODIES APPROVED MODELS**

- a. All competing cars will be full-sized stock American manufactured passenger car composite bodies as approved by AMS and the NASCAR Late Model Stock guidelines as well as the approved suppliers' specifications manual (AR Bodies/Five Star Bodies). Refer to Appendix #1 for diagrams and body dimensions. No steel bodies allowed.
- b. All bodies, regardless of manufacturer, must be mounted on chassis as dictated by the centerline of the chassis. No twisting or skewing of the body will be permitted.
- c. All body/door panels must be mounted in a straight line with the front fenders and rear quarter panels within a tolerance of 2"
- d. Rocker panels must be mounted perpendicular to the ground. NO fairing.
- e. Under car panning is limited to a width of 34" and can only extend from the nose to the rear of the radiator. Rear spoiler must measure 54" wide by 5" tall and be made of Lexan or aluminum and centered on the chassis. No forward braces allowed. Must be between 50- 60 degrees in angle.
- f. The airbox between the radiator and the nose may not be any wider than 34". No air deflectors of any kind are permitted. All air for blowers or coolers must be pulled from the nose. Air blown to brake rotors only---not tires or tire beads.
- g. Base weight will be a minimum of 3100#, with right side weight at a minimum of 1400# with the driver, his/her safety and comfort gear, and full of fuel.
- h. Maximum tread width of 64 1/2" measured from outside to outside of wheels at spindle height. Use of magnetic steel or aluminum wheel spacers to gain proper tread width is permissible. The thickness of the wheel spacers must be the same, left and right, both on the front and rear wheels.
- i. 1/8" per side will be added for double lip wheels.

### **NEXT GEN FIVE STAR BODIES --- 2020 thru 2025**

#### **FIVE STAR LATE MODEL STOCK CAR BODY**

1. All competing cars will be full-sized stock American manufactured passenger car composite bodies as approved by AMS and the NASCAR Late Model Stock guidelines as well as the Five Star Racecar Bodies specifications manual. Refer to Appendix #2 for diagrams and body dimensions. No steel bodies allowed.
  2. All bodies must be mounted on chassis as dictated by the centerline of the chassis and tread width. No twisting or skewing of the body will be permitted. All body panels must remain as manufactured. Streamlining of the contours of the vehicle to enhance the aerodynamic performance is prohibited.
  3. All side/door panels must be mounted in a straight line with then front fenders and rear quarter panels within a 2" tolerance.
  4. Rocker panels must be mounted and remain perpendicular to the ground. NO flaring
  5. The interior side of the roof at the rear above the rear window mounting bed must be supported by a metal brace(s). The brace(s) may be adjustable but must be secured in place and prevent movement of the roof.
  6. Under car panning is limited to a width of 34" and can only extend from the nose to the rear of the radiator.
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7. "Skid" plates may be used to protect the oil pan. These plates must be manufactured so that at least 50% of the surface is open. These plates can not contact the oil pan.
  8. The airbox between the nose and the radiator may not be any wider than 34". No air deflectors of any kind are permitted. All air for blowers and coolers must be pulled from the nose. Air blown to brake rotors only---not tires or tire
  9. Maximum tread width of 64 1/2" measured from outside to outside of front wheels at spindle height. Use of magnetic steel or aluminum

spacers to gain proper tread width is permissible. The thickness of the wheel spacers must be the same on each side, left and right, both on the front and rear wheels.

10. 1/8" per side will be added for double lip wheels.
11. Right side door brace must run at an angle from the top of the right side door sill down to the top of the top roll gage bar---no boxing.
12. Tape may be used ONLY on the front grill and rear spoiler only.

#### OVERALL VEHICLE WEIGHT/HEIGHT

VEHICLE ENGINE	MINIMUM TOTAL VEHICLE WEIGHT	MINIMUM RIDE SIDE WEIGHT
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Ford D347SR Crate engine	3100 lbs.	1375 lbs.
General Motors "Harrington Enforcer"	3100 lbs.	1375 lbs.
General Motors "Upgrade"	3150 lbs.	1450 lbs.
General Motors 88958604/19318604	Crate 3050 lbs.	1375 lbs

1. The "official" scales for each Event are the scales designated by AMS Officials. It is the responsibility of each race team to ensure that its vehicle meets the minimum weight requirements per the Series- designated scales.

- A. The use of non-magnetic and/or hollow fasteners and component mounting hardware with the intent of reducing weight will not be permitted.
- B. All added weight must be of lead, securely held in place in the vehicle's chassis frame rails, be painted white, and have the car number plainly painted on it. NO tungsten. Additional weight may be added to the front subframe lower suspension mounting crossmember behind the front spindles and also to the rear suspension crossmember. This added weight must be contained in a 1/8" minimum thickness magnetic steel box securely bolted in place. NO weight trays will be permitted.
- C. Any weight not secured and lost on the racing surface will result in a \$25/per pound fine to the race team.
- D. Overall vehicle height shall be determined with the car's frame sitting on 4" blocks with the driver in the car. The pushing down or lifting up of the vehicle to meet the body heights will not be permitted.

#### FRONT AIR DAM/REAR SPOILER

1. Front air dam must have a minimum of 4" ground clearance.
2. On all approved 2007-2019 models the leading edge of the air dam may not extend more than 3 inches forward of the bumper measured at the centerline of the front bumper cover. On all approved 2020-2024 models the leading edge of the air dam may not extend more than 4-3/4" forward of the bumper measured at the centerline of the front bumper cover.

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3. On all 2007-2019 approved models the leading edge of the air dam may not exceed 46" when measured from the centerline of the right front spindle. For the 2020-2024 approved models, this measurement is 47".
- 3 Front air dam extensions are permissible providing that the extension is made of flexible plastic or a plastic -like material, be no more than 3/16" thick, be flush mounted with the air dam, and be mounted parallel to the bumper cover.
5. Rear spoiler must be made of aluminum with a minimum thickness of 1/8" or clear polycarbonate with a minimum thickness of 1/4". The rear spoiler shall be mounted in such a manner that the spoiler is centered on the rear of the vehicle.
6. On all 2020-2024 models the rear spoiler must be five (5) inches in height and must be 64 1/2" inches in width. On all 2007-2019 models the rear spoiler must be five (5) inches in height and must be 54" inches in width.
7. On all 2020-2024 models the rear spoiler the maximum height from the ground to the top of the spoiler shall be 39 7/8"

8. The angle of the rear spoiler shall be no less than 50 degrees and no more than 60 degrees. All spoiler heights and angles will be measured with the driver in the car.
9. The spoiler shall consist of two (2) identical blades, left and right, mounted with a 5/8" slot between them. The spoiler blades must be mounted so that the 5/8" slot is located in the center of the vehicle. (i.e. the distance from the end of the spoiler to the edge Each blade must have three (3) mounting bolts attaching the blade to the deck lid and three (3) spoiler braces attached to the rear of the blade.

## **APPROVED ENGINES**

### **A. CRATE ENGINES**

#### **1. Chevrolet Crate (#88958604 and #19318604) (AMS Limited 604 Motor is Legal)**

**B.** Use of 1.6 aluminum self-aligning rocker with 3/8 stud approved on GM 604 crate\*

#### **2. Ford Crate (#D347SR7)**

The approved spec cams for the Ford Crate D347SR7 are the F303 and COMP FPC-D347SR.

Crate engines must be used as supplied by the manufacturer and/or per the specifications manual.

The use of a 1" (maximum) aluminum spacer plate placed under the carburetor will be permitted. Must be four holed or open design. No tapered or bevel designs allowed. No adapters will be allowed.

### **B. SPEC ENGINES**

#### **1. HMS Enforcer**

A. HMS "Enforcer" engine must adhere to guidelines as outlined in HMS Enforcer Specification Handbook. B. NO unapproved modifications, lightening, or changes of any kind to the HMS/ENFORCER parts will be permitted.

C. All engine parts laser labeled with HMS/ENFORCER along with "OR-CODE" labeling must not be disturbed and must be readable at all times

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D. The spec cam shaft as supplied as part of the HMS Enforcer package must measure as supplied by COMP Cams spec sheets supplied by COMP Cams.

#### **2. CHEVROLET UPGRADE SPEC ENGINE**

A. All Chevrolet Upgrade Spec engines must follow the guidelines as defined in NASCAR Rule Book.

B. Edelbrock #2975 Victor Jr. or the Edelbrock #2701 intakes are the approved intakes. Maximum of 1.6 ratio rockers utilized.

## **Air Cleaner**

Minimum of 12" and maximum of 14" (O.D.) air element and housing may be used. Elements must be at least 1 1/2" and no more than 4 inches in height. NO spraying or soaking of elements. Dry element only.

## **Exhaust/Headers**

Exhaust headers are permitted. The headers must be made of magnetic steel and have tubes with a maximum outside diameter of 1- 5/8" and a maximum of 30" in length. Maximum outside diameter of the collector tube is 3". All four (4) header tubes must enter into one collector tube at the same point on each side. NO "merge" collectors or collector "spikes" allowed. "Tri-y" headers are NOT approved. Internal coating of the headers is not permitted.

Exhaust pipes must have an outside diameter of at least 3" and a maximum of 4". The diameter of the pipe chosen must be the same for the entire length of the pipe.

Any device used to reduce or enlarge the inside diameter of the exhaust pipe is not permitted.

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## **Carburetor**

**The only accepted carburetors for competition are as follows:**

**Holley 650 CFM four (4) barrel, Part # 80541-1 ---"Enforcer"**  
**Ford 347SR**  
**GM "604" series**

**Holley 650 CFM four (4) barrel, Part # 80541-2 -----"Enforcer"**  
**Ford 347SR**  
**GM "604" series**

**Holley 650 CFM four (4) barrel, Part # 80541-3 -----"Enforcer"**  
**Ford 347SR**  
**GM "604" series**

**The Chevrolet Upgrade Spec engine must use the following carburetors:**

**Holley 500 CFM-HP, two (2) barrel, part #80583-1**

**Holley 500 CFM-ULTRA HP, 2 barrel, part #4412HB**  
**Holley 500 CFM-ULTRA XP, 2 barrel, part #4412HBX**  
**Holley 500 CFM-ULTRA HP, 2 barrel, part #4412BK**  
**Holley 500 CFM-ULTRA XP, 2 barrel, Part #4412BKX**

Main body --No polishing, reshaping, grinding or plugging of any holes.

Choke plate-- May be removed but screw holes must be permanently sealed. Choke horn may be removed but it must be flush cut with the air cleaner ring.

Boosters---May be changed must be the same type. Size and shape must not be altered. The height and location of the boosters must remain as manufactured. Booster casting must be visible and must remain as manufactured. Boosters must be wired together and held with a small amount of epoxy so as to prevent a drop of the booster into the throttle shaft and throttle plate areas.

Venturi-- Must not be altered or reshaped. Must remain a circular (round) cross section. The casting ring must not be removed.

Base plate---Must be used as manufactured. The position of the throttle bores in the throttle plate must remain as manufactured. The throttle bores must be completely round and straight without taper from top to bottom. Base plate may not be altered in shape or size. All vacuum holes must be threaded and plugged or sealed.

Butterflies May not be thinned or tapered. Idle holes may be drilled in butterflies.

Throttle shafts Must remain stock and may not be thinned or cut in any manner.

Metering blocks Only metering blocks specific to the carburetor being used is acceptable. Number of holes and passages, and their location, in the block must remain as manufactured. Adding or plugging of holes and passages will not be permitted.

Accelerator pump Only Holley replacement parts specific to the carburetor in use will be permitted. Retaining screw may not be drilled for a discharge passage. The pump diaphragm must be 30cc and remain as manufactured. A hole no larger than .050 in diameter may be drilled in

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accelerator pump fuel passage on float bowl side of metering block above fuel level to relieve siphoning through the nozzles.

NOTE: All cars must be equipped with a minimum of two (2) throttle return springs.

### Carburetor Spacer/Gaskets

1. For the General Motors Harrington "Enforcer" engine and the GM crate "604" series engines—a one- piece, solid, open aluminum carburetor spacer 1" in thickness with a one- piece non-metallic gasket with a maximum thickness of .065 inches will be permitted.
2. For the GM "Upgrade" engine: --a one piece solid, aluminum two (2) hole tapered carburetor spacer with a maximum thickness of 1" with a two (2) hole non-metallic gasket with a maximum thickness of .065 inches is permitted.

### Engine Location

1. Engines must be mounted so that the centerline of the crankshaft is in the centerline of the frame, front sub-frame and tread width of the car.
2. All approved engines must be located so that the center of the forward most spark plug hole on the right side is in line with or a MAXIMUM of 1" forward of the center of the right front upper ball joint .

### Flywheel—Flexplate/Clutch/Transmission/Driveshaft/Rear Axle

1. **Only a magnetic steel flywheel mounted to the crankshaft. Minimum outside diameter of the starter ring gears will be 12 7/8" for General Motors and 13 1/4 for the Ford engine**
2. **As an option, the following flywheels will be permitted:**

## Tilton Engineering 51-052-5 (104T) Early General Motor

## Tilton Engineering 51-053-5 (104T) Late General Motors (Neutral) Tilton

## Engineering 51-054-5 (104T) Late General Motors ( Ext. Balance) Tilton

## Engineering 51-055-5 (104T) Ford (Small block)

3. Manual shift transmissions only with four (4) forward gears and a reverse gear--- all operable. Standard production OEM type Muncie or T-10 transmissions are approved as is the top-loader transmission.
4. 4<sup>th</sup> gear-----ratio of 1:00-1. No gear ratios between 1:00-1 and 1:23-1 will be permitted for the three remaining forward gears.
5. Only OEM steel, angle cut gears may be used. No straight cut gears.
6. One -piece, steel driveshaft required. Driveshaft must be painted white and be either 2 ¾ or 3” in diameter. Two (2) steel driveshaft safety loops required, minimum 2" x 1/4", completely around the driveshaft and securely mounted.
7. The minimum clutch diameter is 5 1/2”. Steel multi-disc clutches are permitted up to 3 discs. Only conventional clutches mounted to the flywheel are approved. No carbon fiber, “slipper” or “centrifugal” clutches are permitted.
8. Only one- piece steel axles will be permitted. No crown type axles.

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9. Only metal drive plates are permitted. These plates must be the same thickness left and right. Drive plates must be one piece with a single internal spline.
10. The following differentials will be permitted for use:
  - Detroit locker ratchet type ----- Locked rear drive axle assemblies
11. Rear axle housing support bars or alignment bars will not be permitted.
12. . Cambered rear axle housings or rear axle housings with “toe” will have a tolerance of 1 degree. This measurement will be taken with the car on 4” blocks.
13. Rear axle housing must be centered between the main frame rails of the chassis. Tolerance will be +/- ½”.
14. Only quick-change rear end center sections with a minimum cross section height of 12 inches at the center of the rear axle with a side bell minimum diameter of 12 inches and magnetic steel spur gears on the back side will be permitted.

## Cooling System

1. Radiator must be stock appearing and be mounted in a standard location.. Installation of air directional devices, baffles, dividers and shields will not be permitted. All air that enters the grill area must flow through the radiator core.
2. Radiator must be mounted in the standard vertical position and be within 2” of being perfectly perpendicular to the ground.
3. **NO pressurized coolant system other than the radiator cap.** No Schrader valves or auxiliary access ports are permitted in the cooling system.
4. OEM type water pumps only, impeller may be modified. Shroud may be installed not wider than radiator and a maximum of 1" behind fan blades.
5. May use an electric fan, crank driven fan or no fan.
6. ONLY water may be used in the cooling system. Any additive to the water ( i.e. water wetter) must be approved by the CARS Tour Tech Director. **NO** antifreeze may be added to the cooling system.
7. Portable cooling systems/devices are not permitted.

## Ignition/Electrical

1. Electronic distributors are permitted and must mount in stock location, maintain stock firing order, be gear driven, and be equipped with magnetic pick-up. No crank trigger, adjustable timing mechanisms, magnetos or computerized systems. Single or dual point camshaft driven distributors are also permitted. No magnetos permitted.
2. Only one (1) ignition amplifier box will be permitted and must be mounted on the dash on the passenger side of the car---- out of driver's reach. Any wires from distributor to amplifier box must be run separately---- not part of a loom of wiring harness. NO modifications to the amplifier box will be permitted.
3. The mag positive and negative wiring shall be a maximum of 62" long. All wiring must be point to point and each wiring connection must be easily traceable and removable from the vehicle for inspection. All tachometer wires must be uninterrupted between the tachometer and the supply source. Any splices or bare/punctured wires will not be permitted in the ignition system. Terminated wiring must be sealed to prevent connection. Only **CLEAR** shrink wrap, tape, or bands are permitted in the wiring harness.
4. Only one (1) ignition coil is permitted and must be on the engine side of the firewall or inside the car on the ignition system mounting plate.
5. Only one battery may be installed and that battery may carry a MAXIMUM nominal voltage of 12 volts. Battery must be in an enclosed

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battery box located either behind the front spindle in front of the front firewall or in front of rear axle housing behind the rear firewall. Battery box must be mounted inside the outside edge of the frame rails.

6. All electrical wiring must be sealed and must be point to point and easily traceable. Splices, bare and punctured wires will not be permitted in the ignition system. All electrical switches must be mounted in plain sight and correctly labeled.
7. No electronic traction control devices allowed.
8. No onboard computer or automated electronic recording devices of any kind are permitted.
9. All electrical switches must be on the dash within reach of the driver. Additionally, each switch must be operable and clearly marked. An "on off" switch must be clearly marked and located in the center of the dash with the "on" being in the clockwise direction.
10. All vehicles must be capable of starting under their own power. Push starts are permitted by crew members or the track's support vehicles.  
**NO vehicle may be manually pushed on the racing surface during the Event.**

## Fuel System

1. Foam filled bladder type fuel cell with a 22 gauge steel container required. **MAXIMUM** fuel cell capacity is 22 U.S. gallons. Fuel cell must be mounted with minimum 8" ground clearance. A 1" x 1" steel square tubing cradle, two lengthwise and two crosswise, are required completely around the fuel cell. Fuel cradle must be enclosed on four sides and bottom with 24-gauge steel sheet metal. Rear trunk interior area must be fully floored and sealed off to rear firewall and quarter panels forward of leading edge of fuel cell in such a manner that fuel spill in trunk area cannot come in contact with brake rotors.
2. No electric fuel pumps or pressurized systems. One mechanical fuel pump in stock location is allowed.
3. Any fuel line running through the drivers compartment must be painted red and clearly marked "FUEL".
4. Fuel cells older than five (5) years should not be used.
5. Both the fuel filler and the fuel vent tubes must be equipped with check valves to prevent loss/spillage of fuel.. The two (2) types of check valves, steel ball type OR flap type, are equally acceptable.
6. Overflow/vent tube cannot exceed 55" in length and 1 1/4 "inside diameter, measured from the outside end of the vent mechanism to the fuel cell
7. All cars must have a minimum 1/8" thick intrusion plate of steel or aluminum located between the fuel cell container and the rear end. The

plate must be the same length and width of the fuel cell container. A similar plate on the rear of the container is highly recommended.

## Fuel

### 1. Must use track fuel

Each AMS competitor is required to purchase a minimum of 10 gallons of fuel from the on-site vendor at each Event. Each LM competitor is required to purchase a minimum of 10 gallons of fuel from the on-site vendor at each Event.

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## Brakes

### 1. Operating 4-wheel hydraulic brakes required at all times. Single piston disc brakes with stock (OEM) type calipers will be permitted front and rear. Calipers may be steel or aluminum. **As an option, the following brake calipers will be permitted:**

**Wilwood 120-15611 GM D52-R Single Piston**

**Wilwood 120-15612 GM D52-R Single Piston**

**Wilwood 120-10936 GM D52 Dual Piston**

**Wilwood 120-10937 GM D52 Dual Piston**

### 2. No electric actuators for front brake rotors. A maximum of two (2) 3" flexible cooling hoses per front brake caliper. Air inlets may be cut into the lower front bumper cover no larger than 48 sq. inches for each front brake. Electric fans are allowed in front cooling hoses. The rear brake cooling hoses may be branched off from one of the front brake cooling hoses or originate under the vehicle. This hose may not be lower than 4" at any point. One (1) 3" flexible hose is allowed to be directed toward the inside of each rear caliper. This hose must originate at the side of the trailing arm and be no lower than 4" at any point. Brake fluid re-circulatory systems will not be permitted.

### 3. Inline fans in the front of the car must not be lower than the bottom of the sub-frame rails. Inline fans in the rear of the car must be mounted to sub-frame rails or the rear trailing arms and must not be mounted lower than the bottom of the main frame rail or the bottom trailing arms. Mounting of brake cooling components must be acceptable to CARS Tour Officials.

### 4. Air blown on brake rotors---not on tires and tire beads

## A-FRAMES/SPINDLES/HUBS

**1. Length of lower A-frames must be a minimum of 15-1/8" and a maximum of 16-1/8" measured from the center of the ball joint to the centerline of the mounting points. The location of the center of the lower ball joints must be an equal distance from the centerline of the front sub- frame rails (+ or - 3/8")**

**2. Both lower A-frames must be of equal length. (No offset spindles permitted)**

**3. The distance from the centerline of the tread width and frame rails, front and rear. to the front mounting points of the lower A-frames, left and right, must be the same.**

**4. Spindles manufactured from one piece Billet material are not approved.**

**5. Front and rear hubs must have the same dimensions on the left and right side. Offset hubs will NOT be permitted.**

## Suspension

### 1. Allowable tread width is 64 1/2" MAXIMUM measured at the outside of the wheels at spindle height. 1/8" per side will be added for double lipped wheels. Magnetic steel or aluminum spacers only may be used to achieve desired tread width. The MINIMUM wheelbase on either side is 104 1/2" while the MAXIMUM wheelbase is 105 1/2" on either side.

2. Front and rear springs must be either coil or coil-over springs. Limit of one spring per wheel. All coil springs must be constructed of round magnetic steel wire wound in a clockwise direction. The coil spring wire diameter must be the same size throughout from top to bottom and all coils must be active. The use of a combination of spring types on either front or rear is not permitted. (Example: The use of a conventional coil spring on one side and a coil over spring on the opposite side)
  3. One (1) spring per wheel.
  4. Front coil over springs may not exceed a maximum outside diameter of 4 3/4" for the entire length of the spring. The free height of the front coil spring may not exceed 16". All coils must be evenly spaced.
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5. Rear coil over springs must have evenly spaced coils. . Rear coil springs must maintain a minimum outside diameter of 4 3/4" and a maximum of 5 1/4" outside diameter. The free height of the rear spring must not be more than 16". Rear coil spring lower mounts must be located behind the rear axle housing with the upper mounts welded to the chassis directly above the lower mount.
  6. Spring Pre-loaders will not be permitted.
  7. Front coil springs must have evenly spaced coils and a free height of the bare spring of a maximum of 10-1/2". Front coil springs must have a maximum outside diameter of 5-3/4" and a minimum of 5-1/4". Progressive or digressive rate springs are not permitted.
  8. Rear coil springs must have a free height of the bare spring with a maximum of 16" and a minimum of 11 inches. Both upper and lower spring mounts must be inside the rear frame rails. The rear coil spring lower mounts must be located in front of the rear axle housing. Rear coil springs must maintain a minimum of 4-3/4" and a maximum of 5-1/4" outside diameter.
  9. External travel limiting devices (bump stops) will be permitted on the front suspension only---one (1) on the right and one (1) on the left. 10. NO bump stops on the rear.
  11. Any device(s) such as chains, cables, etc. that limit the travel of the suspension either up or down will not be permitted. When jacking the vehicle, a minimum of two (2) inches of chassis movement is required before movement of the axle/tire assembly

### SHOCK ABSORBERS

1. One (1) shock absorber per wheel.
2. Approved shocks are:

Penske 7500 Series

Bilstein ----- AS2 Series and XVA Series

Advanced Racing Suspensions----- 4000 Series

JRI ST/08 Series

3. All shocks must be revalvable, rebuildable, gas pressurized, monotube, deflective disc valve type with an integral gas reservoir. 4. Single adjustable shocks are permitted

## SWAY BAR

1. Main body of the front sway bar must be a one-piece solid magnetic steel bar mounted perpendicular to the front sub frame rails and centered under the front subframe.
2. The nominal length of the sway bar will be 37-1/2 while the maximum inside diameter of the sway bar shall be 3/4" for the entire length of the bar.
3. Steel sway bar ends and arms only. **MAXIMUM** diameter of sway bar splined ends is 1 3/4". Sway bar arms must be the same length left and right.

## TRAILING ARMS

Only a conventional two (2) link trailing arm type with the same configuration on both sides or a three (3) link passenger car type will be permitted. All link trailing arms must be made of solid construction. No adjustable links allowed. Bushings for truck arms that, in the judgment of CARS Tour Officials, allow excessive vertical or horizontal movement will not be permitted.

1. The following types of conventional trailing arms are permitted:

Truck trailing arms

I-Beam style trailing arms

2. Both arms must be within 1/4 in. of each other in length when measured from the center of the front mounting monoball to the center of the rear axle tube. MINIMUM length of trailing arms is 45". The MAXIMUM length is 51". Mounting of the arms to the chassis in front must be done using monoballs only mounted in the welded sleeve of the arms. Monoballs must be the same size on both sides. Mounting points on the rear axle housing must be evenly spaced and welded to prevent movement and must be equal distance from the longitudinal centerline of the rear frame rails.
3. Passenger vehicle type trailing arms have a maximum of 25" in length measured at the center of the mounting holes. Trailing arms must be constructed of 1-1/4" by 2" steel box tubing with a minimum wall thickness of 1/8". The 2 trailing arms must be equal length and parallel to each other. Mounting points on the rear axle housing must be evenly spaced and welded to prevent movement and must be equal distance from the longitudinal centerline of the rear frame rails.
4. The rear axle housing must be held in the center of the vehicle, side to side, by a one-piece straight tubular track bar connected to the frame on the right side and on the rear axle housing on the left. Moveable threaded-screw adjusters will be permitted on the track bar.
5. Any component or device that will permit the movement or rotation of the rear end housing will not be permitted. U-bolts that hold the truck arms in place must be kept tight at all times.

## Chassis Construction

All chassis construction including, but not limited to, all measurements/ dimensions, thickness of material used and the location/ inclusion of all components of the unit must conform EXACTLY to the rules as stated in the 2024 NASCAR Rule Book.

1. Only 15” diameter, 10” wide, 5 studded steel wheels are permitted. All wheels must have the same width and the same offset. All studs and lug nuts must be of magnetic steel. NO bleeder valves allowed. All wheels must have the car number displayed on the outside half of the wheel.
2. Hoosier F45 approved tires only.
3. All tires designated for use as “race tires” must be bought the day of the Event from the tire vendor and held in a “Series controlled” impound area.
4. Tires designated by the team to be “race tires” will **remain in** the “series controlled” impound area until the car is IN LINE for pre-qualifying technical inspection.
5. Tires may not be treated or altered in any manner. Additionally, tires may not be heated in any manner.
6. **NO** tire rating equipment allowed in the pit area at any time during an Event.

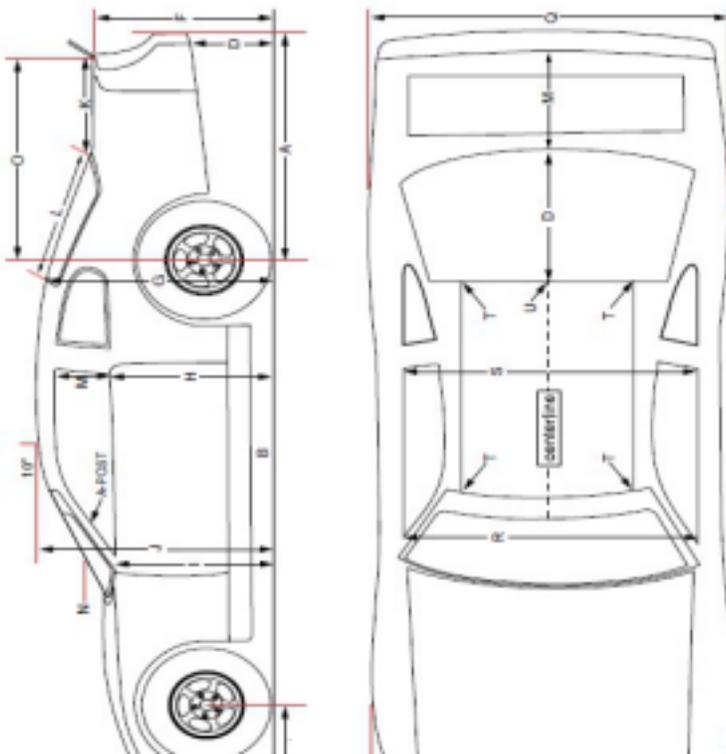
# LMSC

## Late Model Stock Car

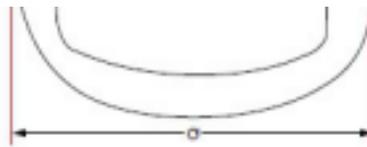
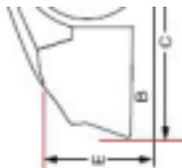
- Fits Late Model Stock perimeter chassis
- 48" roof height
- Maximum tread width: 64-1/2"
- Wheelbase, plastic fenders: up to 106"  
composite fenders: up to 108"
- Approved for use in 

### DIMENSION GUIDELINES

A	REAR OVERHANG (MAX): base of spoiler at centerline to axle centerline	52"
B	NOSE/SIDE PANEL CLEARANCE (MIN): from ground	4"
C	FRONT OVERHANG (MAX): on centerline	46"
D	BUMPER COVER HEIGHT (MAX): from ground	15 1/2"
E	NOSE HEIGHT (MIN): at nose & hood seams	23"
F	QUARTER PANEL/BUMPER COVER HEIGHT: at bumper cover/deck lid intersection	34 1/2"



## A Late Model Stock Car



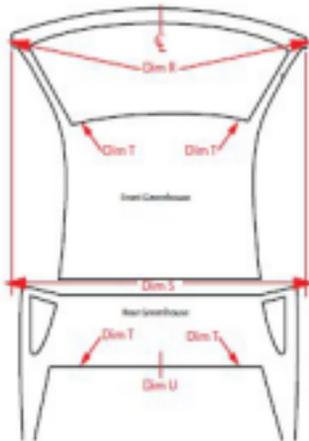
G	ROOF HEIGHT, REAR (MIN): at center rear edge	45 1/2"
H	DOOR HEIGHT: at rear	33"
I	FRONT FENDER HEIGHT: at A-post (MAX)	33 1/2"
J	ROOF HEIGHT (MIN): 10" back from windshield, on centerline	48"
K	DECK LID LENGTH (MAX): at center, from base of spoiler to rear window	23 1/2"
L	REAR WINDOW LENGTH: on centerline	31 1/2"
M	SIDE WINDOW OPENING (APPROX): at B-post	15"
N	WINDSHIELD ANGLE: at center/at roof windshield post	26°
O	REAR OVERHANG (MAX): base of spoiler at centerline to axle centerline	47"
Q	BODY WIDTH (MAX): at wheel wells must not extend past tires	
R	DOOR TO DOOR WIDTH (measured through car): at A-posts & inside edges of doors	67"
S	DOOR TO DOOR WIDTH (MEASURED THROUGH CAR): at B-posts and inside edges of doors	65 1/2"
T	ROOF HEIGHT, CORNERS: at body lines, front	45 1/2"
T	ROOF HEIGHT, CORNERS: at body lines, rear	44 1/2"
U	ROOF HEIGHT, rear: at centerline	45 1/2"

## ROOF MOUNTING

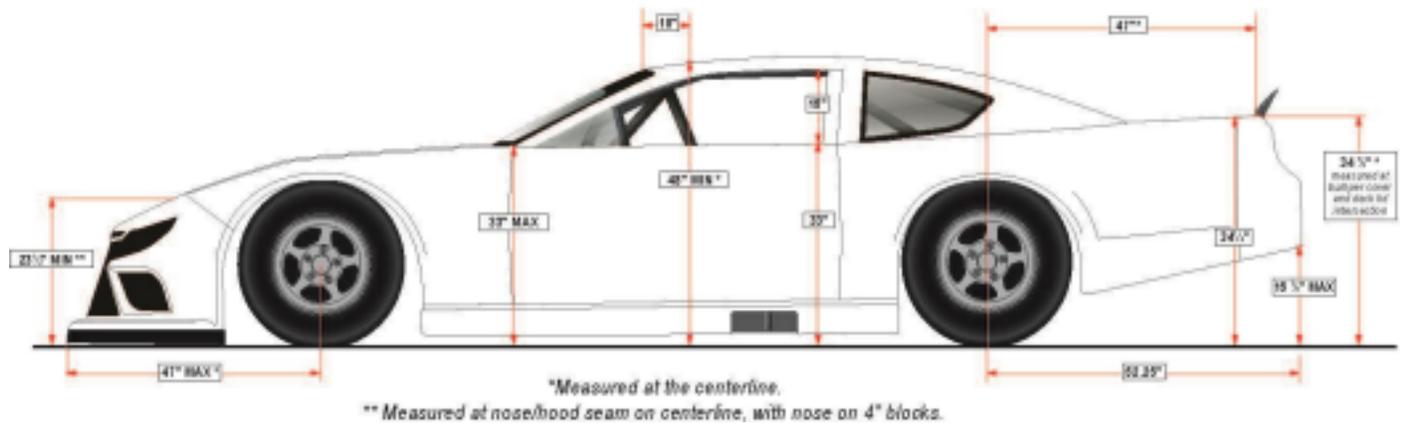
1. Properly locate your roof using the dimensions in the chart at right.
2. Confirm that Dim. J, measured 10" back from the windshield, on the centerline is 48".

### NOTES:

1. DIM R is measured at the A-posts and the inside edges of the doors.
2. DIM S is measured at the B-posts and the inside edges of the doors.
3. The T and U dimensions are taken at the edge of the roof (not on the ledge where the window sits) and are measured from the floor up. Look for the scribe line indicators in the window bed for the locations to measure dimension T.



# FIVE STAR RACE CAR BODIES 2020 LATE MODEL STOCK BODY



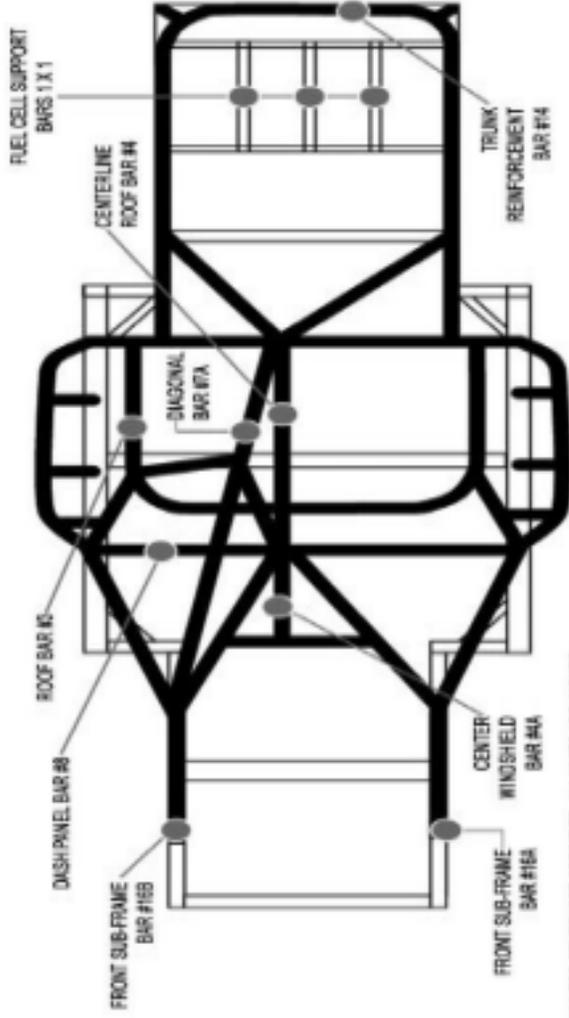
31

## Late Model Stock Car Diagram Appendix #3

### Chassis Construction

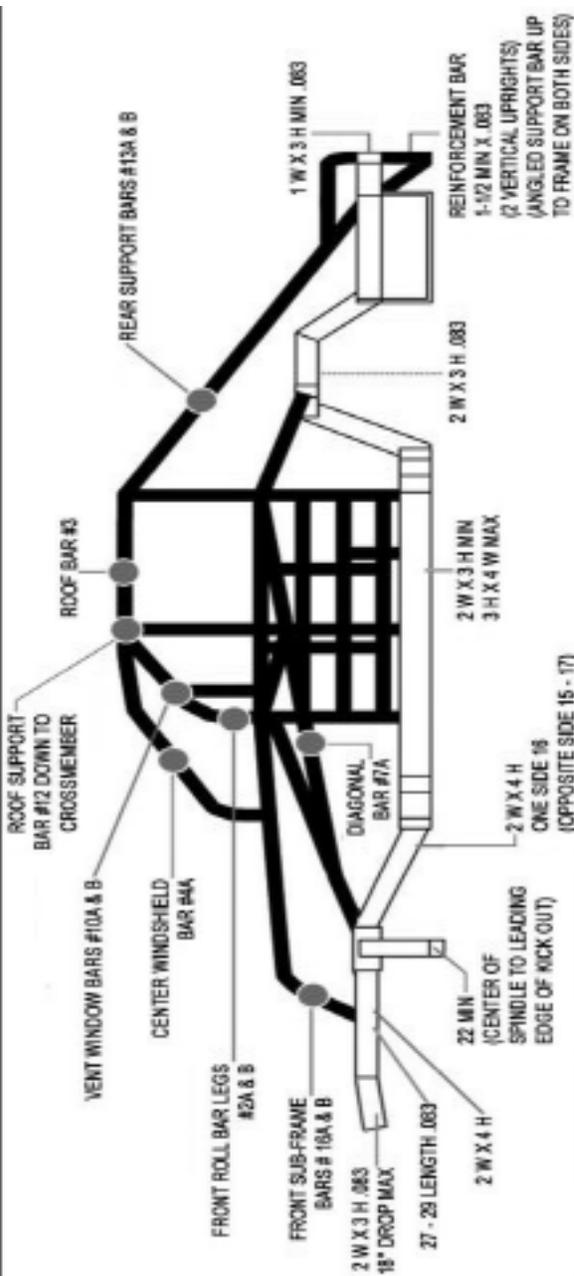
### Appendix # 3

**DIAGRAM #2 - TYPICAL ROLL CAGE AND FRAME CONSTRUCTION (PLAN VIEW)**



NOTES: UNLESS OTHERWISE SPECIFIED, ALL UNITS ARE INCHES.

**DIAGRAM #3 - TYPICAL LATE MODEL ROLL CAGE & FRAME CONSTRUCTION**



NOTES: UNLESS OTHERWISE SPECIFIED, ALL UNITS ARE INCHES. 22-25 DEGREES RISE .083

# DIAGRAM #4 - EXPLODED VIEW OF BASIC ROLL CAGE

(SOME BARS REMOVED FOR CLARITY)

